

Version 1.0

Owner's Manual STEELER 52S Performance



Steeler yachts
Wheermaten 16
8331 TL Steenwijk
(The Netherlands)

Contents

1. GENERAL	5
1.1. INTRODUCTION	5
1.2. ABOUT THIS MANUAL.....	6
1.2.1. Warnings.....	6
1.2.2. Schematic drawings.....	6
1.2.3. Manuals	6
1.2.4. Pictures.....	7
1.3. NAUTICAL PHRASES AND CONVERSION FACTORS	7
2. GENERAL PARTICULARS	9
2.1. PRODUCER	9
2.2. TYPE AND IDENTIFICATION.....	9
2.2.1. WIN (formerly CIN/ HIN)	9
2.2.2. Builder's plate.....	9
2.3. DESIGN CATEGORY	10
2.4. DIMENSIONS, WEIGHTS AND SPECIFICATIONS.....	11
2.5. DECK LAYOUT	11
2.6. INTERIOR LAYOUT	12
2.7. ENGINE ROOM, LAZARETTE AND TECHNICAL AREA	12
2.8. TANK ARRANGEMENT.....	13
2.9. PAINT SYSTEM/ ANODES.....	13
2.9.1. Paint system	13
2.9.2. Antifouling.....	13
2.9.3. Anodes	13
2.10. STABILITY.....	14
2.10.1. Maximum load	14
2.11. SPEED RESTRICTIONS.....	15
3. EMERGENCY AND SAFETY (PROCEDURES)	16
3.1. WORKING DECK.....	16
3.2. EXPLOSION HAZARD.....	17
3.3. ABANDONING SHIP.....	17
3.4. LIFESAVING EQUIPMENT	17
3.5. ESCAPE ROUTES.....	18
3.6. FIRE : PRECAUTIONS	18
3.7. FIREFIGHTING EQUIPMENT	20
3.7.1. FIRE !!	20
3.8. CARBON MONOXIDE	21
3.9. BATHING PLATFORM.....	21
3.10. PERSON OVERBOARD: UNINTENTIONALLY	22
3.11. PERSON OVERBOARD: INTENTIONALLY: SWIMMING	22
3.12. FLOODING, SWAMPING OR CAPSIZING	22
3.13. COLLISION.....	23
3.14. GROUNDING.....	23
3.15. LEAKS	23
3.16. EMERGENCY STEERING	23
3.17. TOWING	24
3.18. MEDICAL EMERGENCY	25
3.19. SLIDING WINDOW	25
3.20. HOOKING POINTS/ JACK LINES.....	25
4. OPENINGS IN HULL, DECK AND SUPERSTRUCTURE.....	26

4.1.	THRU-HULL FITTINGS	26
4.1.1.	<i>Hull-penetrating sensor</i>	<i>27</i>
4.2.	WINDOWS/ HATCHES/ DOORS	27
5.	DECK EQUIPMENT	28
5.1.	DECK-WASH SYSTEM	28
5.2.	ANCHOR WINCH	28
5.3.	CAPSTANS	29
5.4.	TRANSOM HATCH	29
5.4.1.	<i>Hydraulic tender winch</i>	<i>30</i>
5.5.	MAST	30
5.6.	GANGWAY	31
5.7.	LIFTING ROOF	32
6.	STEERING SYSTEM	33
6.1.	STEERING SYSTEM	33
6.2.	BOW- AND STERN THRUSTER	34
7.	PROPULSION	35
7.1.	DIESEL ENGINES	35
7.1.1.	<i>Engine control</i>	<i>35</i>
7.1.2.	<i>Engines cooling</i>	<i>36</i>
7.1.3.	<i>Main engines exhaust</i>	<i>37</i>
7.1.4.	<i>Propeller and shafting</i>	<i>37</i>
7.2.	FUEL SYSTEM	37
7.2.1.	<i>Filling the fuel tanks</i>	<i>38</i>
7.2.2.	<i>Diesel filtration</i>	<i>38</i>
7.2.3.	<i>Fuel degrades</i>	<i>39</i>
7.2.4.	<i>Emergency cut-off valves</i>	<i>40</i>
7.2.5.	<i>Tank level indicator</i>	<i>40</i>
8.	ELECTRICAL SYSTEM	41
8.1.	CENTRAL SWITCHBOARD	41
8.2.	LAY-OUT OF DC SYSTEM	41
8.2.1.	<i>Charging batteries</i>	<i>42</i>
8.2.2.	<i>Navigation lights</i>	<i>43</i>
8.2.3.	<i>Hull isolation control</i>	<i>43</i>
8.2.4.	<i>Fuses DC</i>	<i>43</i>
8.2.5.	<i>Main switches</i>	<i>43</i>
8.2.6.	<i>Tips and advice for DC systems</i>	<i>44</i>
8.3.	LAY-OUT OF AC SYSTEM	44
8.3.1.	<i>Shore-power</i>	<i>45</i>
8.3.2.	<i>Isolation transformer</i>	<i>46</i>
8.3.3.	<i>Inverters</i>	<i>46</i>
8.3.4.	<i>Generator</i>	<i>46</i>
8.3.5.	<i>Circuit breakers AC</i>	<i>47</i>
8.3.6.	<i>Tips and advice for AC systems</i>	<i>47</i>
8.4.	NAVIGATION EQUIPMENT AND DOMESTIC APPLIANCES	47
9.	HYDRAULIC SYSTEMS	48
9.1.	CENTRAL HYDRAULIC SYSTEM	48
10.	WATER- AND WASTE SYSTEMS	49
10.1.	FRESH WATER SYSTEM	49
10.1.1.	<i>Pressure system</i>	<i>49</i>
10.1.2.	<i>Filling the tank</i>	<i>50</i>

10.1.3.	Hot water	50
10.1.4.	Tank level indicator.....	51
10.2.	WASTE WATER SYSTEM	51
10.2.1.	The black water tank	52
10.2.2.	The grey water tank.....	52
10.2.3.	Heads	53
11.	TRIM AND STABILIZER SYSTEM	54
11.1.	TRIM SYSTEM	54
11.2.	STABILIZING SYSTEM	54
11.2.1.	Stabilizer cooling system.....	55
12.	VENTILATION, AIR CONDITIONING AND HEATING.....	56
12.1.	AIR-CONDITIONING SYSTEM.....	56
12.1.1.	Collection tank.....	57
12.2.	VENTILATION ENGINE ROOM.....	57
13.	BAILING.....	58
13.1.	ELECTRICAL BILGE PUMPS.....	58
13.2.	BACKUP BILGE PUMP	59
14.	MAINTENANCE.....	60
14.1.	THE INTERIOR	60
14.2.	THE EXTERIOR	61
14.2.1.	Saltwater corrosion	61
14.2.2.	Maintenance of paint	61
14.2.3.	Antifouling paint.....	61
14.3.	TECHNICAL SYSTEMS MAINTENANCE/ TROUBLE SHOOTING/ WINTERIZATION	61
14.3.1.	Winterising the ship / laying up: general.....	62
14.3.2.	Engines and generator.....	62
14.3.3.	Engine won't start	62
14.3.4.	Steering system	62
14.3.5.	Fuel system.....	62
14.3.6.	Fresh water system.....	63
14.3.7.	Air-conditioning	63
14.3.8.	Wastewater systems	63
14.3.9.	Electrical appliance does not work.....	63
14.3.10.	Battery chargers.....	64
14.3.11.	Bailing system	64
14.3.12.	Fire fighting equipment	65
14.3.13.	Thru-hull fittings and seacocks.....	65
14.3.14.	Thru-hull sensors	65
14.3.15.	Galvanic corrosion.....	66
14.3.16.	Non-return valves	66
14.3.17.	Plexiglas: windows, hatches, port lights.....	66
14.3.18.	Stainless steel.....	67
14.4.	HAULING UP/ CRADLING	67
14.5.	CHECKLISTS	67
14.5.1.	CHECKLIST "DAILY"	68
14.5.2.	CHECKLIST "PERIODICALLY"	69
15.	NAVIGATING YOUR SHIP.....	75
15.1.	STARTING AND STOPPING THE ENGINE.....	75
15.1.1.	Manoeuvring	76
15.1.2.	Mooring.....	76
15.1.3.	Anchoring	76

15.2.	SPEEDS / RANGE	77
16.	ENVIRONMENTAL CONSIDERATIONS.....	78
16.1.	LEAKAGES OF FUEL AND OIL	78
16.2.	HOUSEHOLD WASTE	78
16.3.	CLEANING AGENTS	78
16.4.	NOISE	78
16.5.	WAVES.....	79
16.6.	EXHAUST EMISSIONS.....	79
16.7.	PAINT	79
17.	RECEIPT OF DOCUMENTS.....	80
18.	DECLARATION OF CONFORMITY	81
19.	NOTES	83
20.	ANNEXES.....	84
20.1.	SUPPLIED MANUALS	85
20.2.	WARRANTY DOCUMENT	86
20.3.	ELECTRICAL DIAGRAMS.....	87
20.4.	SUPPLIED SCHEMATIC DRAWINGS.....	88

Changes in specifications can be made without due notice.

No rights can be obtained from this manual.

Final responsibility for the contents of this manual lies with the producer of the item which is the subject of this manual.

The producer will not accept responsibility for damages, direct and indirect that result from the literal text of this manual, when reason and common sense should prevail.

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1. General

1.1. Introduction

Congratulations on the purchase of the 'STEELER 52S'.

This manual has been compiled to help you to operate your craft with safety and pleasure.

It contains details of the craft, the equipment supplied or fitted, its systems and information on its operation.

Please read it carefully, and familiarise yourself with the craft before using it.

Even when your boat is categorised for them, the sea and wind conditions corresponding to the design categories¹ A, B, and C range from strong gale to severe conditions, open to the hazards of a freak wave or gust, and are therefore dangerous conditions, where only a competent, fit and trained crew using a well maintained boat can satisfactorily operate in such conditions.

Ensure that the anticipated wind and sea conditions will correspond to the design category of your boat, and that you and your crew are able to handle the boat in these conditions.

This owner's manual is not a course on boating safety or seamanship. If this is your first craft, or you are changing to a type of craft you are not familiar with, for your own comfort and safety, please ensure that you obtain handling and operating experience before "assuming command" of the craft.

Your national sailing federation or yacht club will be pleased to advise you of local sea schools, or competent instructors.

This owner's manual is not a detailed maintenance or trouble shooting guide. In case of difficulty, refer to the boat builder or its representative. Always use trained and competent people for maintenance, fixing or modifications. Modifications that may affect the safety characteristics of the craft shall be assessed, executed and documented by competent people. The boat builder cannot be held responsible for modifications he has not approved.

NOTE Any change in the disposition of the masses aboard (for example the addition of a radar, a stowing mast, change of engine, etc.) may significantly affect the stability, trim and performance of your boat

Users of this boat are advised that :

- all crew should receive suitable training;
- the boat should not carry more than the manufacturer's recommended load;
- bilge water should be kept to a minimum;
- stability is reduced by any weight added high up;
- in rough weather, portholes, windows, hatches, lockers and doorways should be closed to minimise the risk of flooding;
- stability may be reduced when towing or lifting heavy weights using a davit or boom;
- breaking waves are serious stability hazard.

In some countries a sailing licence or authorisation are required, or specific regulations are in force; check beforehand.

Always maintain your boat properly and make allowance for the deterioration that will occur in time and as a result of heavy use or misuse of the boat.

Any boat – no matter how strong it may be, can be severely damaged if not used properly. This is not compatible with safe boating. Always adjust the speed and direction of the craft to sea conditions.

¹ Categories are used within the scope of the European Boating Directive

2. General particulars

2.1. Producer

The 'STEELER 52S' is produced by
Address

: Steeler Yachts
: Wheermaten 16
8331TL Steenwijk
The Netherlands
: +31-(0)521-510268
: www.steeleryachts.com
: info@steeleryachts.com

Phone
Internet
E-mail

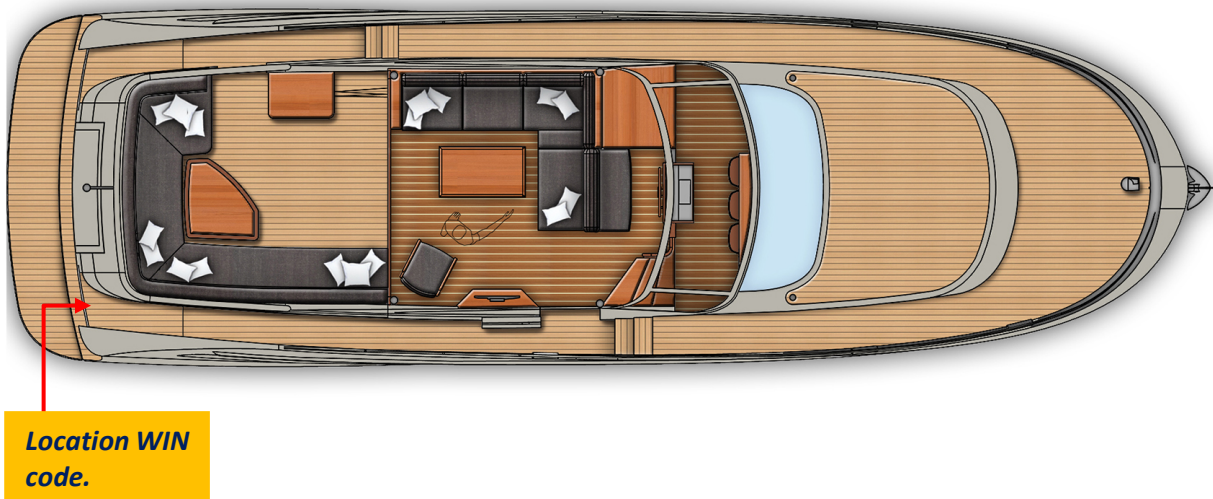
2.2. Type and identification

2.2.1. WIN (formerly CIN/ HIN)

The data that are combined into the WIN (**W**atercraft **I**dentification **N**umber) (formerly known as CIN or HIN) are amongst others:

- producer
- type of boat
- building month and year
- model year

The WIN is affixed on the upper starboard aft side of the boat (lower step of the staircase to the bathing platform). The WIN code of your specific yacht can also be found on the "declaration of conformity" behind §18 of this manual.



CAUTION:

Do not cover, damage or alter the WIN.

2.2.2. Builder's plate

The builder's plate is affixed at the dashboard, well visible from the steering position.

This plate contains valuable information on the 'STEELER 52S', and may not be removed.
The information on this plate contains amongst others:

- name of builder / producer;
- CE-mark;
- design category;
- maximum recommended number of persons when under way;
- maximum recommended load (excl. tank capacities).



WARNING:

- Always put the safety of the crew and the ship on number one.
- Do not exceed the maximum recommended number of persons. Regardless of the number of persons on board, the total weight of persons and equipment must never exceed the maximum recommended load.
- Always load the craft carefully and distribute loads appropriately to maintain design trim (approximately level). Avoid placing heavy weights high up.
- Sea fasten all loose items.
- Adjust your speed and course to the conditions and other traffic.
- Do not make any sudden or unexpected maneuvers to prevent anyone losing their balance or footing.

2.3. Design category

The design category is assigned to the boat by the producer of the boat.

The 'STEELER 52S' is assigned **category A** but bear in mind that the speed of the yacht has to be adjusted to the sea state (see § 2.11).

These categories are defined in the European Boating Directive, and have the following definitions:

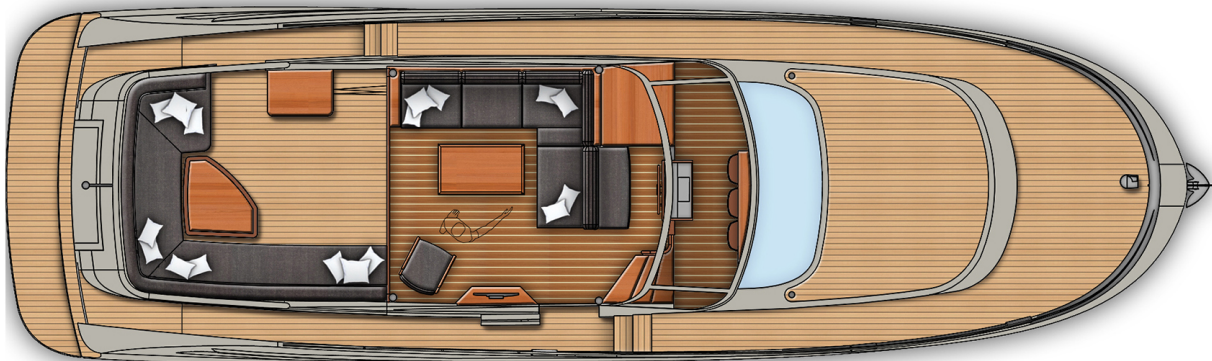
- | | |
|----|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| A: | A recreational craft given design category A is considered to be designed for winds that may exceed wind force 8 (Beaufort scale) and significant wave height of 4 m and above but excluding abnormal conditions, such as storm, violent storm, hurricane, tornado and extreme sea conditions or rogue waves. |
|----|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
- B: A recreational craft given design category B is considered to be designed for a wind force up to, and including, 8 and significant wave height up to, and including, 4 m.
- C: A watercraft given design category C is considered to be designed for a wind force up to, and including, 6 and significant wave height up to, and including, 2 m.
- D: A watercraft given design category D is considered to be designed for a wind force up to, and including, 4 and significant wave height up to, and including, 0,3 m, with occasional waves of 0,5 m maximum height.

Your 'STEELER 52S' is built to exact standards, and complies with the essential requirements from the Directive. Herein are laid down the requirements for stability, buoyancy, construction, installations etc. The ISO standards which are followed to comply with these requirements are listed in the Declaration of Conformity, refer to § 18.

2.4. Dimensions, weights and specifications

Building material	Aluminum		<i>remark</i>
Length of the hull (L _H)	14.99	m	
Length over all	15.68	m	
Beam hull	4.70	m	
Draught (approx.)	1.2	m	
Air draught (approx.)	3.26	m	Mast down, roof down
Air draught (approx.)	3.42	m	Mast down, roof up
Air draught (approx.)	4.3	m	Mast up, roof down
Air draught (approx.)	4.46	m	Mast up, roof up
<i>variations in draught and air draught are approx. 10 cm depending on load and trim conditions</i>			
Displacement (approx.)	18	tonnes	50% loaded
Fresh water (approx.)	750	litres	1 tank
Fuel (approx.)	2.500	litres	2 tanks of 1.250
Black water (approx.)	300	litres	1 tank
Maximum number of persons	6	pers.	on builders plate
Maximum weight	1.060	kg.	on builders plate
Maximum speed (approx.)		knots	<i>to be completed</i>
Engines	2x Cummins QSC 8,3 (447 kW each)		

2.5. Deck layout

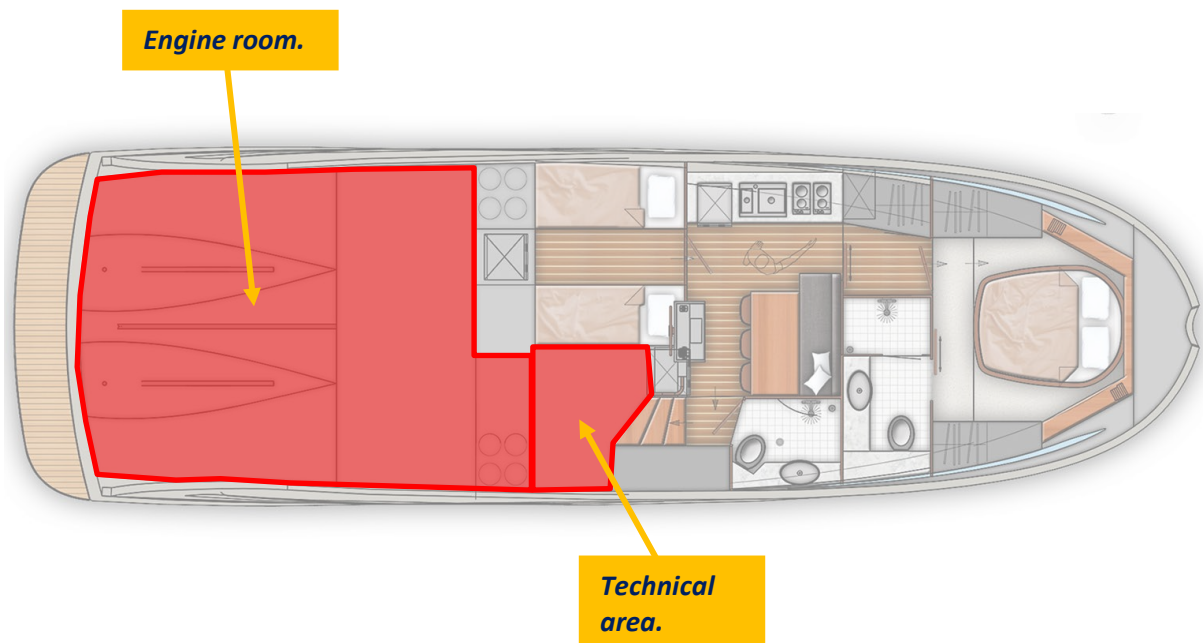


2.6. Interior layout



2.7. Engine room, lazarette and technical area

In this manual we frequently talk about the engine room and the technical area. With this we mean:



The technical area can be entered by lifting the staircase from the front of the yacht to the salon. The engine room can be entered from a hatch in the aft deck.

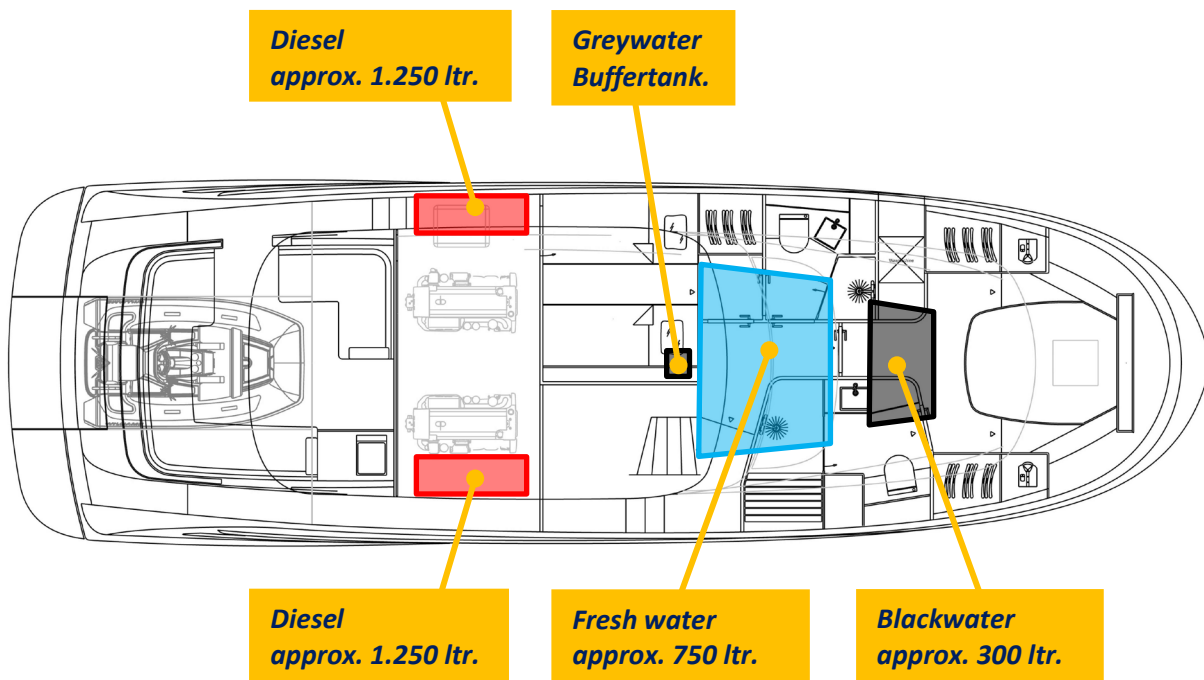


WARNING:

- The technical area and engine room are high risk areas due to moving parts, loud noise, high-temperature and electrified parts.
- Keep the technical area's closed when operating in rough weather and only enter if absolutely necessary.
- Before entering the high risk areas, make sure you are not wearing clothing that may cause accidents, such as loose clothing that may get entangled in moving

parts. Wear protective clothes suitable for the kind of work (shoes, glasses, ear muffs and working overalls). Button up cuffs, avoid using ties or scarves and do not keep long hair loosen.

2.8. Tank arrangement



2.9. Paint system/ anodes

2.9.1. Paint system

A complete two-component paint system has been applied, to achieve a “top class” yacht finish. Remaining paint must be treated as minor chemical waste. Therefore make sure that during painting activities this does not end up in the environment.

The paint system may not be polished during the first five years. All polishing agents deteriorate the structure of the paint such that it will age prematurely.

2.9.2. Antifouling

At delivery, the underwater body has been treated with two layers of anti-fouling paint. The antifouling has to be repainted every year and should be approved for the specific sailing area.

2.9.3. Anodes

The underwater body of the hull is protected by the “standard” sacrificial anodes. When the yacht is hauled up, the anodes have to be checked. Replace anodes that are consumed over 50%.

To the inside of pumps and machinery, like the main engines, there could also be anodes. See manuals (📖) of the equipment for more information on that.

3. Emergency and safety (procedures)

Safe boating means knowledge and training about the vessel you are operating and the area in which you operate. It means knowing the limitations of your yacht, yourself and your crew.

Safe boating means that the Owner / Captain / Operator of your vessel accepts and learns certain basic rules and practices so that your vessel, you, your crew, your guests, other boaters, and the environment are protected.

This owner's manual is not a course on boating safety or seamanship, but in the following section you will find some information relating to the safe operation of your vessel under normal and adverse conditions, safety procedures to implement during normal and distress situations, and information about keeping your environment clean and safe.



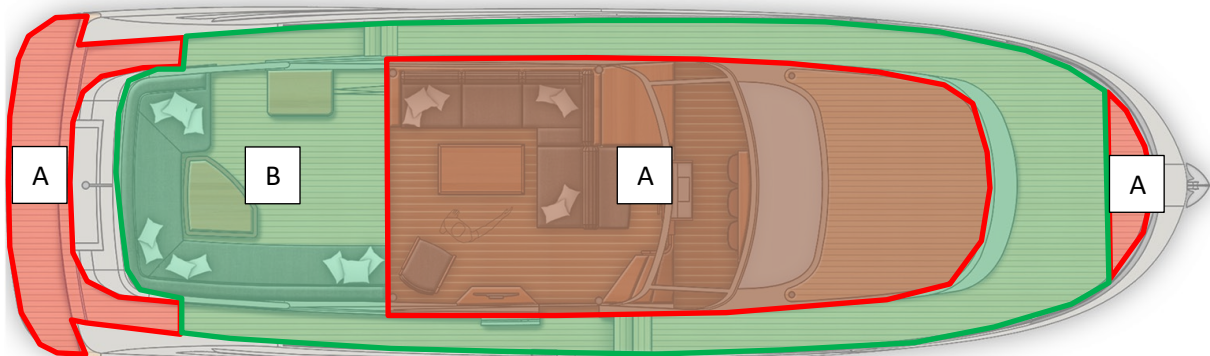
WARNING:

- The time to think about emergencies is before they happen. Plan ahead Know what to do before you encounter any of these situations.
- Wear a PFD when boating.

3.1. Working deck

All persons should wear a suitable buoyancy aid (life jacket/ personal floatation device) when on deck. Note that in some countries, it is legal requirement to wear a buoyancy aid that complies with their national regulations at all times.

The bathing platform, decks of the superstructure and front part of fore deck do not belong to the working deck. The following schematic drawing shows the working area's:



- A. Bathing platform + superstructure area + front part of the fore deck**
RESTRICTED AREA while underway. External area without adequate protection against falling into the sea.
- B. Working deck**
External area where you are protected against falling into the sea, equipped with a railing, good handholds and hooking points. Use safety harnesses and lifelines on this area when conditions require this.

3.2. Explosion hazard

If explosion is imminent, put on PFD's, grab distress signals and survival gear, and immediately abandon ship.

3.3. Abandoning ship

Please read items below as "if and when possible";

- Make a head count, gather everybody in the cockpit.
- If possible, launch life-raft and (depending on the weather and situation) use this to evacuate the ship.
- If possible, launch the tender.
- Gather distress signals (if available), personal belongings, telecommunication tools, GPS, EPIRB, compass (etc.).
- When clear of danger, account (again) for all who were on board, and help those in need.
- Keep everyone together to make rescue easier.

BURN HAZARD – Swim or sail against the current or wind if you abandon ship.

Leaking fuel will float with the current and may ignite.


3.4. Lifesaving equipment

The 'STEELER 52S' can be fitted with a life-raft (owner's delivery). This raft can be stored in the cockpit area. Have rafts checked annually.



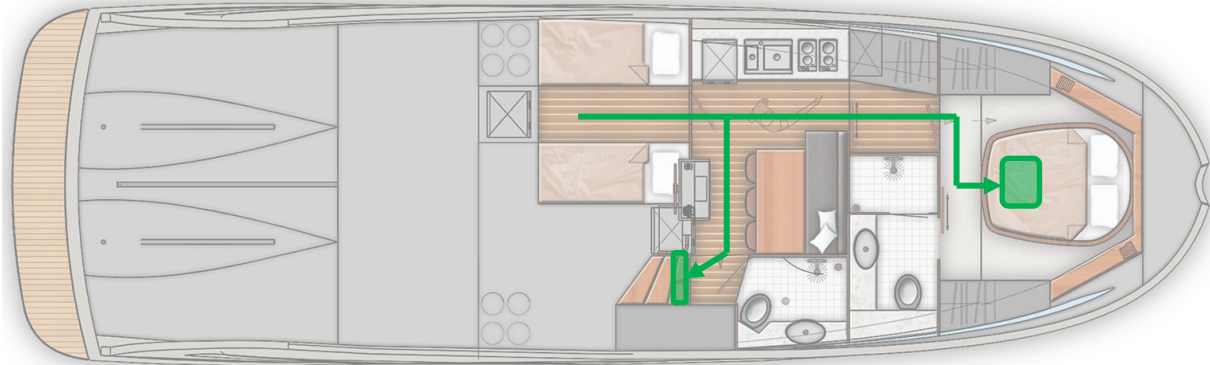
DANGER:

It is the responsibility of the owner / skipper to have sufficient lifesaving equipment on board for all that sail with you, and to instruct anyone on board of their location, their use and the relevant procedures for their use.

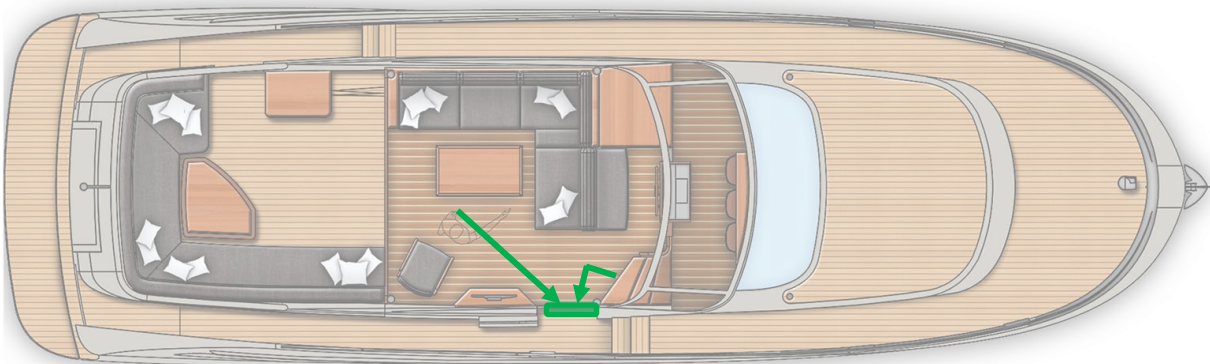
Refer to the supplied manuals  with the equipment.

3.5. Escape routes

The lower deck of the yacht has two escape routes. One is up the normal stairs to the main deck and the other one leads through the escape hatch on the front deck (in front of the superstructure).



Escape routes.



From the upper deck, the normal entrance door at SB of the wheelhouse is regarded as an emergency escape door. The door from the salon to the aft deck has to be kept close while at sea and is not considered an escape door.



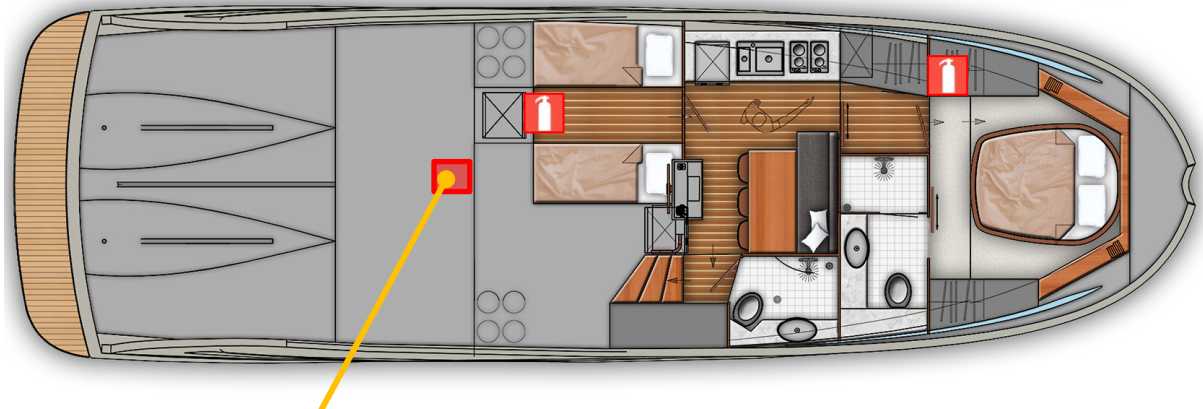
WARNING:

- The door from the salon to the aft deck should be kept closed while at sea;
- Never block passages in the accommodation or escape-routes;
- Never block access to safety controls.


3.6. Fire : precautions

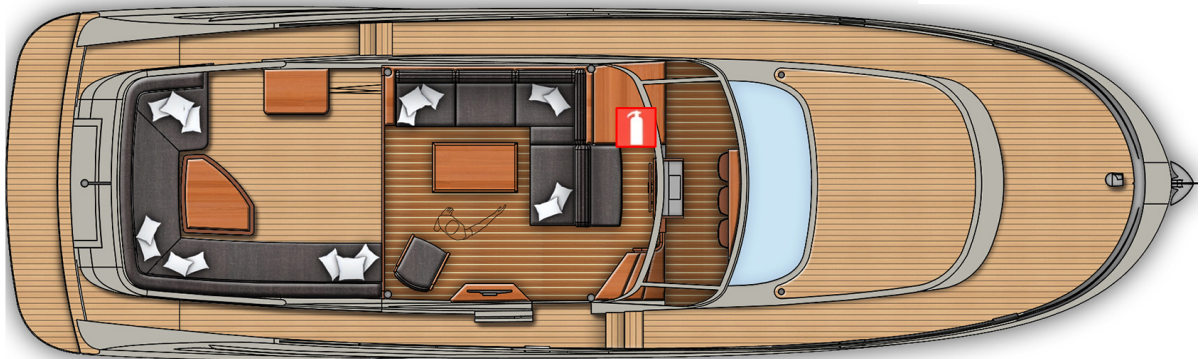
We sincerely hope you will never have to use the fire extinguisher on board the 'STEELER 52S' but it is for your safety and for the safety of those on board, that good care is taken of the extinguishers, and that all the necessary instructions are given to crew and guests.


The extinguishers are fitted in the following locations:



Fixed fire-extinguisher.

 :portable extinguisher



 :portable extinguisher



WARNING:

NEVER:

- store an extinguisher in the back of a locker, out of easy range;
- leave the ship unattended when cooking appliances are switched on;
- alter systems on board without expert assistance;
- smoke when handling gas, petrol, diesel or other lightly flammable goods;
- store combustible goods in the engine room or technical area (and always sea-fasten everything).

It is the responsibility of the owner/skipper to:

- have firefighting equipment checked at intervals indicated on the equipment;
- replace firefighting equipment, if expired or discharged, by devices of identical or greater firefighting capacity;
- inform members of the crew about:
 - the location and operation of firefighting equipment;
 - the location and operation of the fixed fire extinguisher for the engine compartment;
 - the location of escape hatches/ escape routes;
- ensure that the firefighting equipment is readily accessible when the craft is occupied
- keep the bilges clean and check for fuel and gas vapours at regular intervals;

- when replacing parts of the firefighting equipment only matching components shall be used, bearing the same designation or being equivalent in their technical and fire resistant capabilities;
- combustible materials shall not be stowed in the engine compartment.

3.7. Firefighting equipment

The 'STEELER 52S' is equipped with fire-fighting equipment as follows:

LOCATION

for engine compartment

1 fixed fire extinguisher (automatic and manual control);

accommodation:

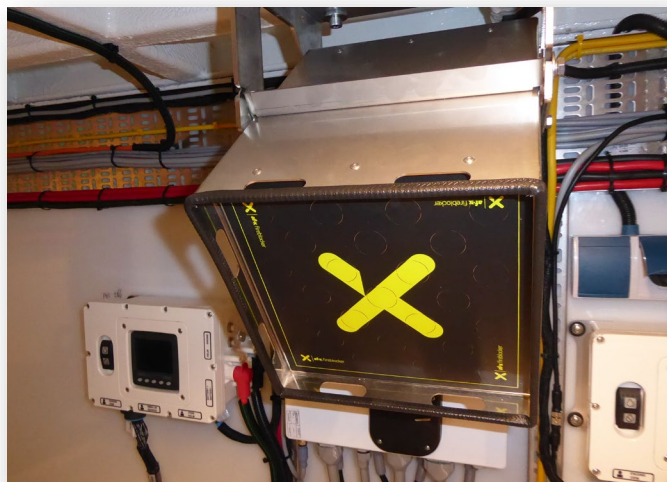
3 pcs, minimum 5A/34B.



CAUTION:

Foam extinguishers may not be exposed to frost. The contents can freeze, possibly rupturing the cylinder.

*Fire extinguisher
engine room.*



The fixed fire extinguisher (📖) for the engine room is automatically and/ or manually operated. The panel of this extinguisher can be found in the hallway of the lower deck (back of the steering position). If the fixed extinguishers goes off, an alarm will sound in the wheelhouse and one of the two lights underneath the steering position will start to flash.

3.7.1. FIRE !!

In case the worst happens and fire starts:

Fire in the accommodation:

- Evacuate the area, make a head count.
- Turn air conditioning, and ventilation systems OFF.
- Delegated task: Inform all aboard of the situation and inform the authorities. Signal for help.
- Delegated task: Prepare for the worst and start emergency procedures for evacuation.
- Delegated task: Put on PFD's, grab distress signals and survival gear, and prepare to abandon ship.

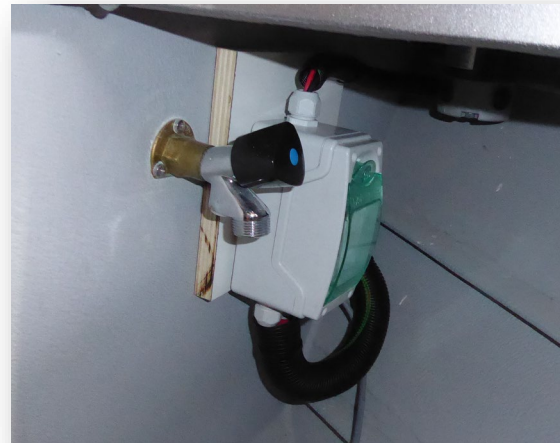
5. Deck equipment

5.1. Deck-wash system

Your yacht is fitted with two dedicated deck-wash connections near the anchor winch (underneath the PS front hatch).

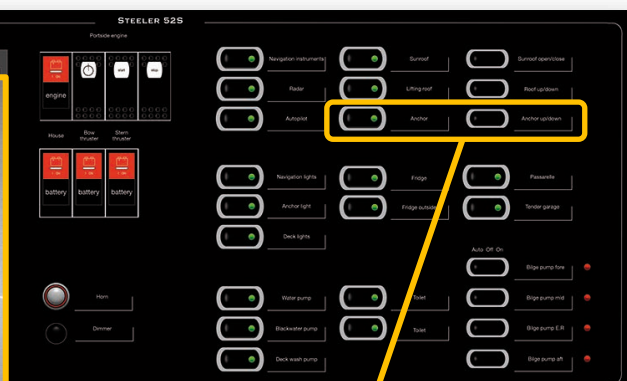
One of them is supplied by the fresh water system of your yacht (out of the fresh water tank). The other one is supplied with sea water through a deck-wash pump. The thru-hull fitting has to be opened (see § 4.1) and the pump started to get water pressure on this connection. The pump can be started from the wheelhouse dashboard.

One of the Deck-wash connection.



5.2. Anchor winch

There is an electrically powered anchor winch fitted on the foredeck of the 'STEELER 52S'. The anchor winch is fitted underneath the central fore deck hatch. The anchor winch can be controlled from the dashboard on the wheelhouse. You first have to activate the winch with the left button and then it can be controlled with the right button. The anchor winch can also be controlled by a wired remote control from the fore deck.



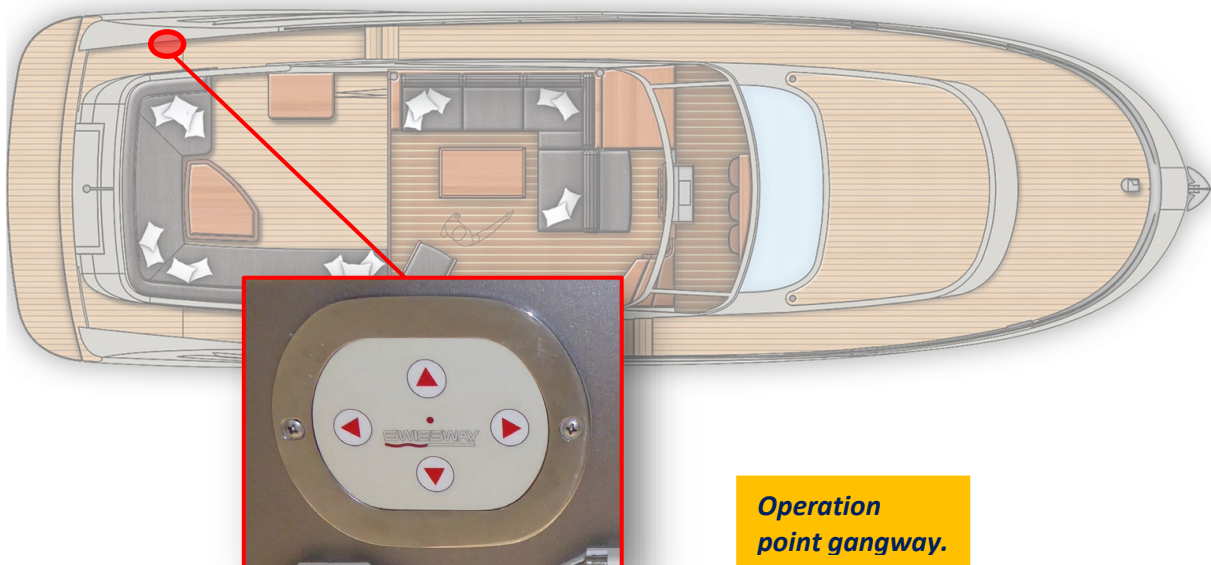
Control buttons anchor winch.

For more details and maintenance, please refer to the manual of the winch (📖). Under the section "Anchoring" (please refer to page 76) you find some general tips and advises on anchoring.

- Never use the hydraulics under the effects of alcohol, drugs or in conditions of physical discomfort without being helped.
- Always announce in advance the intention to operate the mast, especially when there are people on the aft deck.
- Turn the system off when not in use.
- To avoid risk of injury care should be taken to keep hands and limbs clear of lever and lid pinch zones while operating the mast.

5.6. Gangway

On the PS aft of your yacht, the retractable gangway (🚶) has been fitted. This is operated by buttons on the PS aft deck. The gangway has an own hydraulic powerpack, located in the engine room PS aft (see picture at § 9.1).



WARNING:

- Never exceed the safe working load of 150 kg.
- Never use the gangway as a springboard. Diving may cause strong strains that might damage the gangway and its fastening elements.
- The gangway must be used by competent people only. Minors of 16 years or younger and people who might not be aware of the dangers involved with its improper use, should not be allowed to use it.
- Always hold children in your arms when boarding over the gangway.
- Operation must always be carried out under visual control and the operation area must always be well enlightened.
- Never operate the gangway under the effects of alcohol, drugs or in conditions of physical discomfort without being helped.
- Always keep your balance.
- Always announce in advance the intention to move the gangway, especially when there are people about to go on it.
- In rough sea conditions or when the stern and the load are swaying, strong and sudden strains may occur and compromise the stability of the gangway. No